



2020/21 **MODLITES AUSTRALIA INC.**

RULES OF COMPETITION

July 1, 2020

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1 PREFACE

The rules and/or regulations set forth herein are designed to provide for the orderly conduct of racing events and to establish minimum acceptable requirements for such events. These rules shall govern the condition of all events, and by participating in these events, all participants are deemed to have complied with these rules. No expressed or implied warranty of safety shall result from publications of or compliance with these rules and/or regulations. They are intended as a guide for the conduct of the sport and are in no way a guarantee against injury or death to a participant, spectator or official. The race director shall be empowered to permit reasonable and appropriate deviation from any of the specifications herein or impose any further restrictions that in his opinion do not alter the minimum acceptable requirements. No expressed or implied warranty of safety shall result from such alteration of specifications. Any interpretation or deviation of these rules is left to the discretion of the officials and their decisions are final.

2 RULES REVIEW PERIOD

Clubs are encouraged to participate during the rules review period. Send request and recommendations to modlitesaustralia@outlook.com

Rules Review Period 1st of November to 31st December

3 2020 OVERALL RULE CHANGES FROM 2019

3.1 Adoption of Stars Rule Book

3.2 1325lb weight rule

4 GENERAL RULES

4.1 Modlites Australia race rules shall apply to all sanctioned events.

4.2 All vehicles are subject to inspection at any time.

4.3 Approval of a vehicle by the inspector shall mean only that the vehicle is approved for participation in a competitive event and shall not be construed in any way to mean that the inspected vehicle is guaranteed mechanically sound. Be it further declared that the inspector should not be liable, nor shall the sanctioning body, for any mechanical failure not for any losses, injuries or death resulting from same.

4.4 Rules and Procedures for the Modlites Australia drivers can be found by going to the www.modlitesaustralia.com.au/Rules.

4.5 All drivers must be a minimum of sixteen (16) years of age. All drivers under 18 years of age **MUST** have a signed waiver by parent or guardian. All drivers must hold a current Speedway Australia "B" licence this rule will be governed by Speedway Australia regulations.

5 CLASSIC MODIFIEDS & MODLITE (IMCA/UMP/DIRTCar) SPECIFICATIONS

5.1 Any car specifications not covered in these body specs are subject to Modlites Australia official's interpretation. Any significant performance advantage will be addressed on an individual basis. Modlites Australia official's reserves the right to determine what constitutes an unfair advantage.

5.2 1928-1948 American- made production vehicles only, 5/8 scale, two-door vintage coupe, sedan, sedan delivery, wagon, or pick-up truck. These are to be known as Classic Modified body styles.

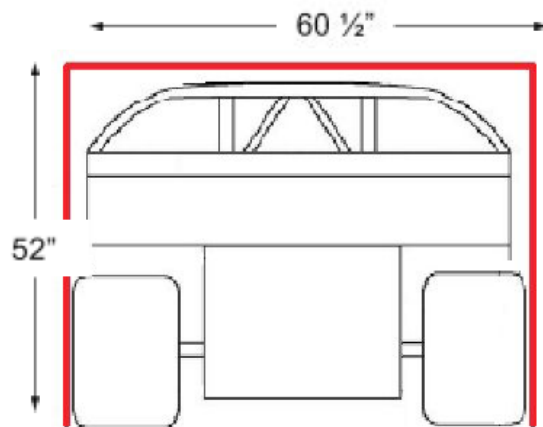
5.3 Modified appearing body style such as IMCA/UMP/DIRT modified. These are to be known as Modified Lite body styles.

5.4 All chassis specifications are the same for all body styles.

5.5 All measurements may be taken with or without the driver and/or with/without fuel tolerance permitted on all body dimensions is maximum of +/- (plus or minus) $\frac{1}{2}$ " –inch (One half inch). This is a tolerance, not a dimension that is intended to be added to the body dimensions.

5.6 The intent of these specifications is to follow the bodyline and design of an exciting IMCA/UMP/DIRT modified racecar. Body must be 5/8 scale, stock appearing. Maximum outside tire width 60", Cars must pass freely through a hoop 60 $\frac{1}{2}$ " wide by 52" high, 73" wheelbase length plus or minus 1". When measuring wheelbase, front tire must be in line with rear tire.

Figure #1



5.7 NO car over 52" tall, top to ground. Body height maximum 46", bottom edge to top (including frame rail).

5.8 Maximum body width 50", Maximum frame width 34" min 30."

5.9 Body contour must follow frame and roll cage. Roof hatch entrance allowed.

5.10 Driver's door must be operational and equipped with secure latch.

5.11 Fasteners on hood, trunk, or panels must be positive, locking type.

5.12 130" maximum total length, bumper to bumper measured from inside to inside of bumper or 132" measured from outside to outside of bumper.

5.13 Kick outs or side nerf bars must be mounted two or three points and run full length between tires. $\frac{1}{4}$ " max .125 wall thickness. Cannot stick out beyond 1" from the side of the tire.

6 CLASSIC MODIFIEDS BODY SPECIFICATIONS

6.1 Must have grill shell or simulated open radiator matching original body style, any material.

6.2 Doors, windows, and cowlings must retain stock appearance. Hood louvers allowed for cooling but must maintain stock contour.

6.3 Engine compartment must conform to scale and match contour of body. Hole may be cut for breather clearance only. Ram air induction system prohibited.

6.4 Must have closed trunk lid or fully enclosed trunk access panel conforming to original bodylines.

Louvers allowed no holes.

6.5 No fenders.

6.6 No wings or spoilers.

6.7 Body will be metal construction, 26-gauge steel or .040" aluminium minimum. Fiberglass or plastic grill shells allowed. All exposed edges must be folded or trimmed over. No lips or spoilers of any kind.

6.8 NO open top cars (roadsters, convertibles). Only closed top, meaning hardtop.

6.9 No new classic or registration that has expired more than 12 months will be permitted after 2015 unless continual registration has been held

7 MODLITE (IMCA/UMP/DIRTCar) BODY SPECIFICATIONS (DIRTCar Figure 1) (IMCA/UMP Figure 6)

7.1 86" max - Door and rear quarter length.

7.2 30" min 38" max - side height of rear quarter panel from the ground. 30" max height of quarter panel.

8 MODLITE (IMCA/UMP/DIRTCar) HOOD AND NOSE SECTION

8.1 20" min 27" max - hood height excluding breather protectors (bottom of frame rail to top of hood).

8.2 Rear of hood becomes part of windshield area rules.

8.3 No part of nose section, 8" in front of the lower ball joint, may be wider than a maximum of 24". Modified Lite DIRTCar design must resemble approved DIRTCar nose.

8.4 Hood may taper out to door panels at the rear.

8.5 Maximum 1.5-inch side fins allowed on aluminium nose.

8.6 IMCA/UMP Body only - approved plastic nosepieces allowed. Plastic nose must be mounted in an approved manner and can extend no further back than front of hood.

8.7 Bumper may not extend more than 30" forward of spindle. Measurement to be taken from either side of car centre of spindle to outside of front bumper.

8.8 Cooling holes allowed

9 MODLITE (IMCA/UMP/DIRTCar) ROOF PANEL (DIRTCar Figure 5) (IMCA/UMP Figure 9)

9.1 40" min 48" max - length of roof panel. 36" min 42" max - width of roof.

9.2 No part of roof panel may have more than 14° of angle rake. No deflector of any type on upper sides or rear of roof panel. Roof entrance hatch optional.

9.3 Maximum .75-inch ridge down sides of roof. (See Figure 12)

9.4 Maximum .75-inch rear roof stiffener (must face down).

9.5 Driver roof hatch Mandatory.

9.6 Maximum 1.5 inch rolled down rock guard allowed on roof front.

10 MODLITE REAR UPPER QUARTER PANEL (Sail Panel) IMCA/UMP/DIRTCar Figure 12 & 13

10.1 20" min 38" max rear upper side panel may contain plexiglass.

10.2 Side panels maybe no higher than 3" at the rear of the straight line of quarter panel top edge.

10.3 Side panel may be no higher than an imaginary line from the rear edge of roof panel to 3" above the top edge of the rear quarter panel.

10.4 Both sail panels will be a mirror image.

11 MODLITE (IMCA/UMP/DIRTCar) SIDE WINDOW (DIRTCar Figure 4) (IMCA/UMP Figure 8) (Mandatory NO TOLERANCE)

11.1 Driver and passenger side windows must have 18" min side opening length.

11.2 10" minimum side opening height front and rear.

12 MODLITE INTERIOR (DIRTCar Figure 10) (IMCA/UMP Figure 11)

12.1 46" min 50" max - width of rear shelf and body panels.

12.2 Rear shelf and cockpit area may have no more than a 5o max rake and may not be concave or convex in design.

12.3 Cockpit areas will not be fabricated similar to the late model style design. No convey or convex decking.

13 MODLITE INTERIOR SPECIFICATIONS (DIRTCar Figure 2) (IMCA/UMP Figure 6)

13.1 DIRT Car only 36" min 56" max - length of right-side pod area with max 30o. Left side pod may be altered for driver access. Side pods not permitted on IMCA/UMP body style.

13.2 Rear quarter distance from centre of rear 20" min 32" max.

13.3 Cockpit cover with a max of ½" lip will be permitted providing the on/off cut-off switch is reachable from outside the car by safety personnel.

13.4 Deck lid height from ground 30" minimum, 36" maximum with NO rear lip extending upward.

13.5 DIRT Side pod may not extend rearward past the centreline of rear axle.

13.6 Body must be centred on chassis.

13.7 If windshield is used, it must be flat

13.8 All body dimensions will be tech'd + or - ½" unless otherwise specified.

14 MODIFIED LITE REAR SPOILER (DIRTCar Figure 10) (IMCA/UMP Figure 11)

14.1 A one piece, aluminium/Lexan spoiler with a maximum height of 4"-inches from the rear deck will be permitted.

14.2 The rear spoiler must be non-adjustable from the cockpit and/or during racing conditions. Hinges, adjuster(s), slides, and/or any other adjusting type device permitted.

14.3 Metal gurney and/or table and/or flanges and/or lips will not be permitted.

14.4 A brake and/or bend on the top of the aluminium/lexan spoiler will be permitted for reinforcement.

14.5 The maximum overall height of the spoiler when measured from the ground must not exceed 40"-inches.

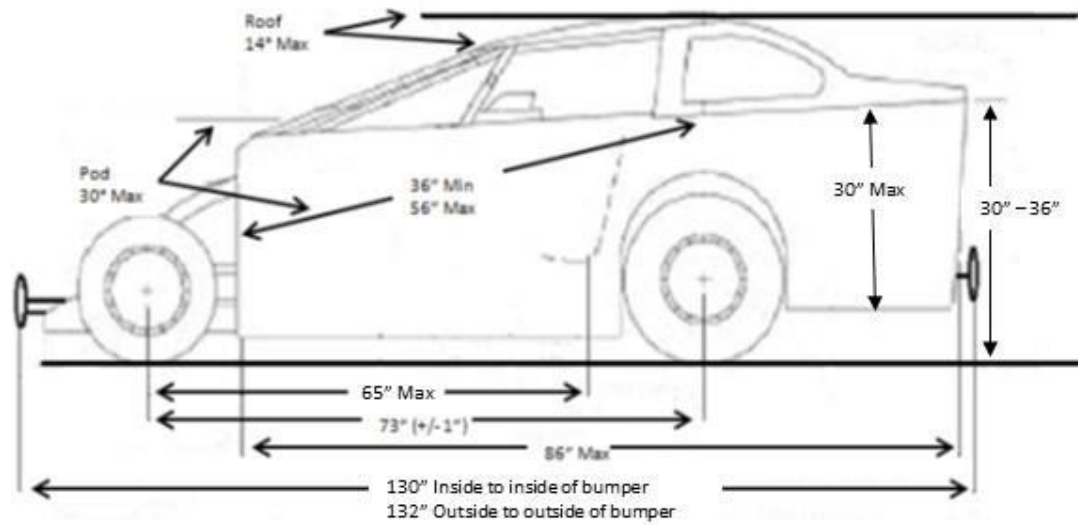
14.6 No vertical supports for the purpose of fastening the spoiler to the rear deck will be permitted.

15 MODLITE REAR FRAME AREA (DIRTCar Figure 10) (IMCA/UMP Figure 11)

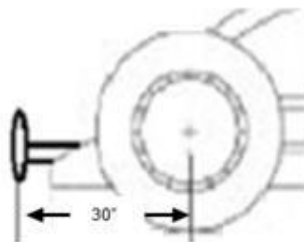
15.1 The rear frame area from the rear shelf downward may have an optional enclosed panel.

15.2 Louvers and holes allowed

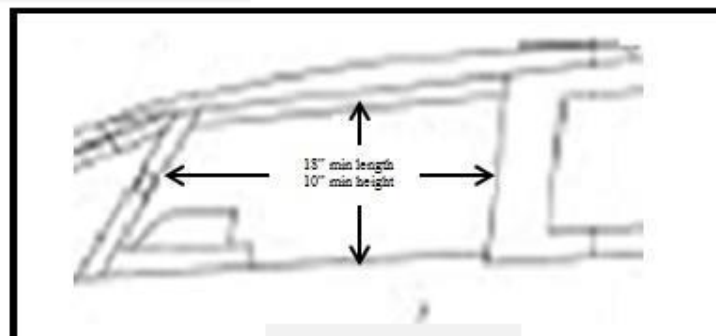
DIRTCar Body Styles



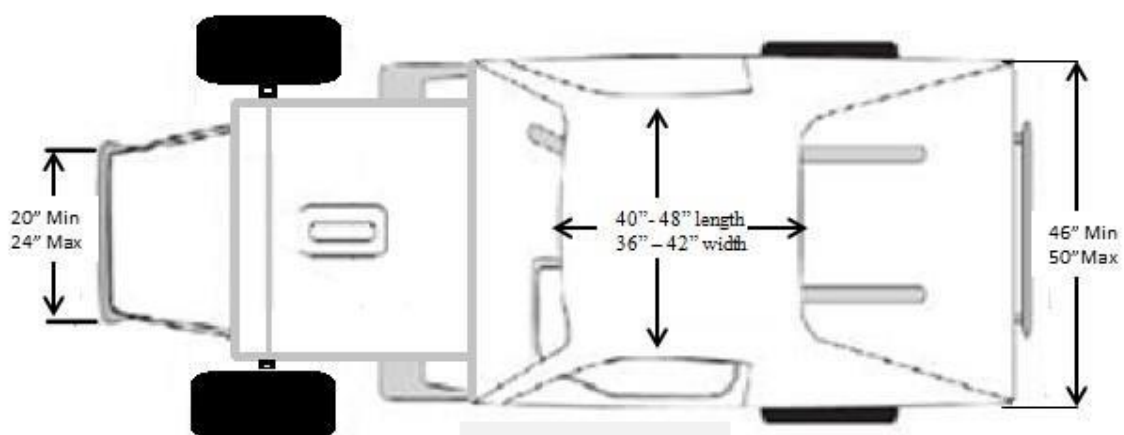
DIRTCar Figure 2



DIRTCar Figure 3

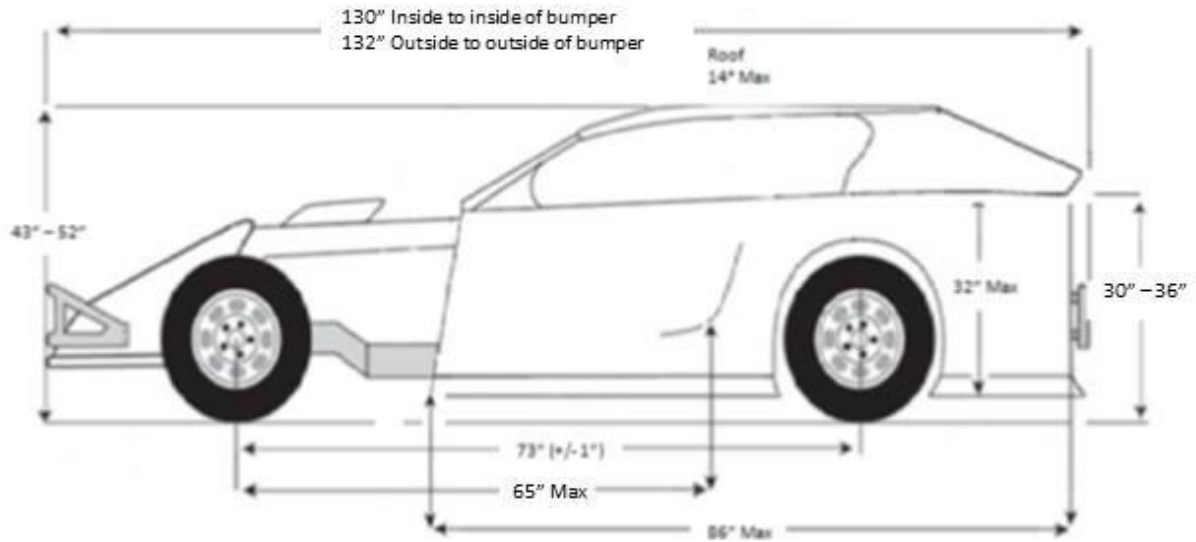


DIRTCar Figure 4

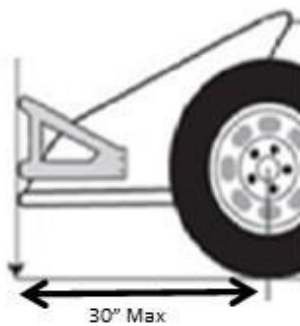


DIRTCar Figure 5

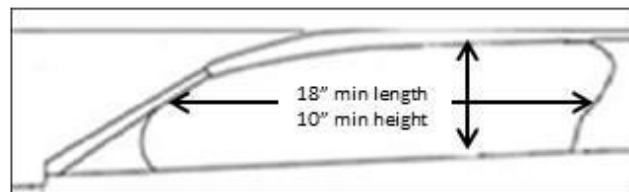
IMCA/UMP Body Styles



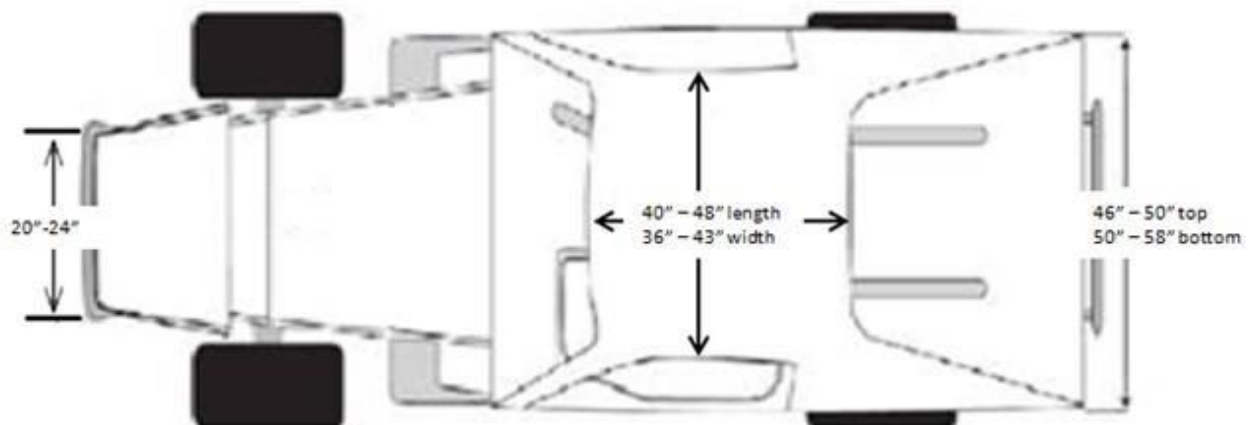
IMCA/UMP Figure 6



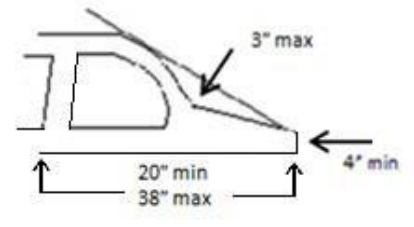
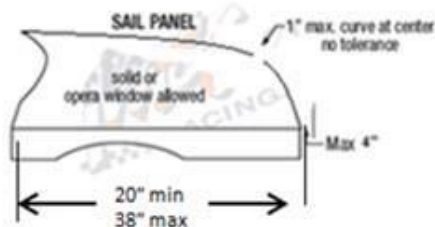
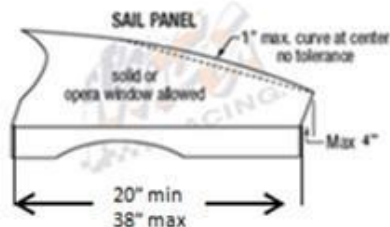
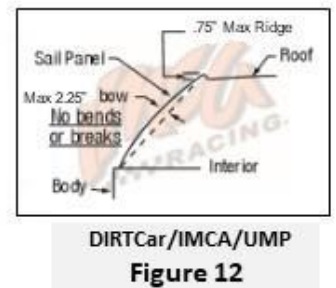
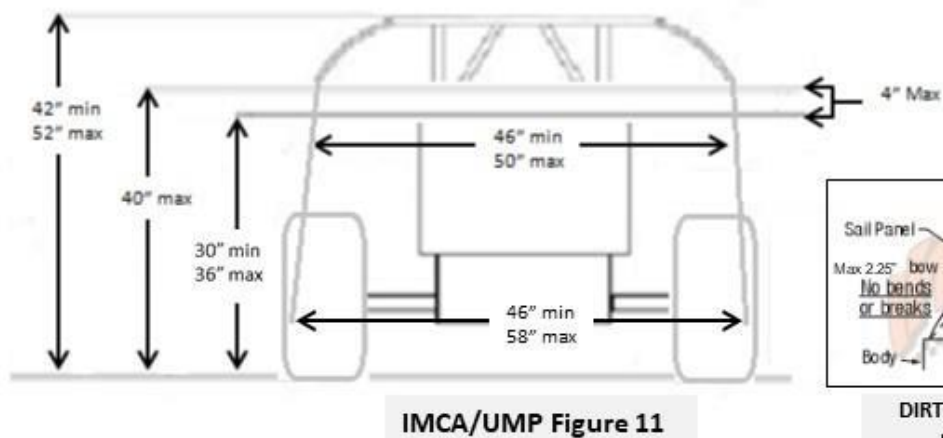
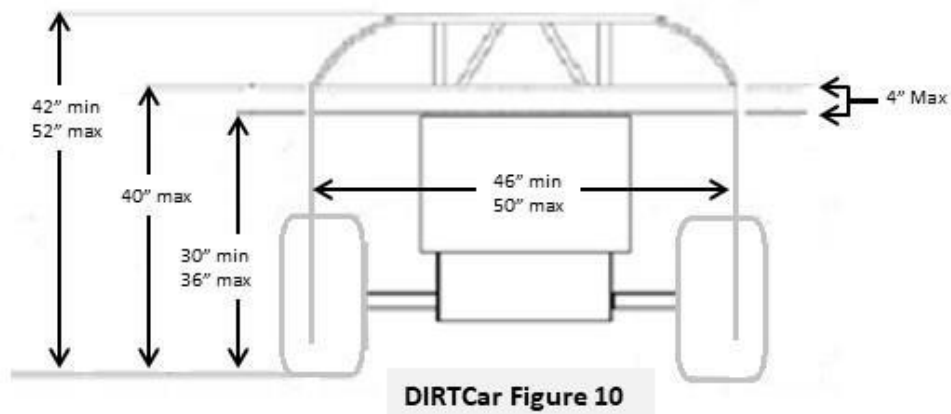
IMCA/UMP Figure 7



IMCA/UMP Figure 8



IMCA/UMP Figure 9



DIRTcar/IMCA/UMP Figure 13

16 WEIGHT PROGRAM

16.1 All cars following the MODLITES AUSTRALIA program must weigh a minimum of 1325 lbs, with driver after race.

16.2 No weight outside of car body, i.e. on nerf bars.

16.3 This weight program will be monitored for parity and competitiveness and may be changed at the discretion of MODLITES AUSTRALIA.

16.4 Attached weight must be securely bolted to frame with 1 (1/2") or larger bolts up to 5kg and 2(1/2") or larger bolts for weights over 5kg. No more than 11.5kgs allowed in a single weight. Weight must be painted white with car number displayed. Penalty for losing attached weight on the racetrack is disqualification from the event.

17 BUMPER DIMENSIONS

17.1 Front maximum width 30", Bumper height 6" to 8" with 2-bar loop. Ground to top of bumper: 12" minimum, 16" maximum. Bumper may not extend more than 30" forward of spindle. Measurement to be taken from either side of car centre of spindle to outside of front bumper.

17.2 Rear maximum width 56" with 3 vertical bars. Bumper height 6" to 8" with 2-bar configuration. Ground to top of bumper: 12" minimum, 18" maximum. May not extend more than 10" behind rear body panels.

17.3 Bumpers must be hollow with max wall thickness of .125

17.4 Front bumper minimum width will be 20"

17.5 To ease pre & post-race inspection easy to remove bumper/nerf bar connecting pins or bolts required.

17.6 A hoop is allowed on the top side of the rear bar no wider than 12" and 8" high made of the same material as the rear bar. This is an option for push car protection only

18 FIREWALL

Firewall must be metal and must completely separate engine compartment from cockpit. Engine protruding into cockpit must be enclosed.

19 WINDSHIELD

Windshield must be, Wire mesh 2"x 2", or protection bars (1"x1" highly recommended.) to be a minimum thickness of 3/32". No Glass. All other cockpit areas must remain open. Deflectors may be used in front of driver. May be no wider than roll bar area.

20 NERF BARS

Full length nerf bars between front and rear tires mandatory. Maximum outside diameter of 1.25" and .125 maximum thicknesses, must be hollow and may not extend out beyond tread width more than 1" at the rear only.

21. SEAT

21.1 Seat must be high-backed aluminium, racing type. Left and right head supports mandatory

21.2 Recommend complete driver full containment type seat with head rest and head surround that meet or exceed the SFI 39.2 specification and display a legible and valid SFI 39.2 label.

21.3 Seat must be mounted to a seat frame that is welded to the racecar frame/roll cage structure. Attaching points, angles, and materials for the seat frame and mounting of the seat to the seat frame must be in accordance to the seat manufacturer instructions.

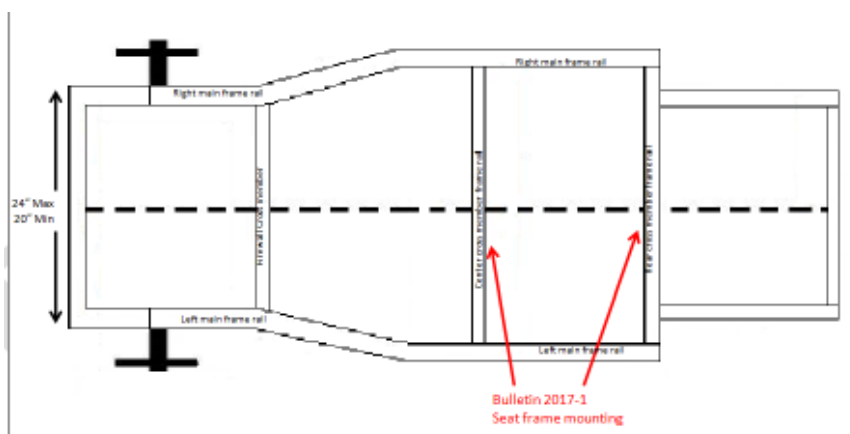
21.3.1 Cars manufactured prior to the 2018 production year with seats bolted directly to the floor will be grandfathered. Floor must be welded in if seat is bolted directly to the floor.

21.4 Seats must be used as supplied and instructed by the seat manufacturer with the exception of trimming the length of the left side head surround for the purpose of egress only. If the left side head surround is trimmed to a distance that is less than the most forward surface of the drivers helmet (usually the area crossing the chin) then recommend a left side head net meeting the SFI 37.1 installed with a quick release latch.

21.5 All cars will have a permanent bar located behind top of seat at shoulder level.

21.6 Bottom of seat will not be more than 65" back measured from upper ball joint.

21.7 Seat may be tilted back for added driver head clearance; however, no portion of the seat may be back more than 74" as measured above



22 NUMBERS, GRAPHICS & TRANSPONDER

22.1 Car number must be minimum 12" in height and 2" stroke. Must be displayed on each door and roof or trunk. Numbers in 4" size should be on front and rear of car to help with line-ups.

22.1.1 State prefix must be in front of all numbers i.e. Q – Qld, S – SA, W – WA, N – NSW, V – Vic, N-NT, T-Tas

22.2 2 Transponder to be located on the rearward side of the front lower arm cross member of the main chassis rails (*between front wheels*)

23 BATTERY, FUEL SYSTEM, RADIATOR

23.1 Battery and fuel pump must be mounted outside driver compartment. Battery must be secure and held with non- rubber strap. Electric fuel pump allowed.

23.2 Fuel cell mandatory, maximum 5 gallons, located in trunk.

23.3 Reinforced or steel braided line must be used in all high-pressure areas.

23.4 A direct manual cut-off switch wired to ground circuit will be installed on the right rear shelf or rear firewall within 6" of the corner formed by the rear deck and rear firewall. Switch will be located over

the right shoulder of the driver. The emergency electrical cut off switch will be clearly located and marked fuel cut off.

23.5 Forward facing radiator hoses must be covered when the radiator is mounted behind the driver's head. Rear facing hoses recommended

24 DRIVE TRAIN

24.1 Must use steel drive shaft from transmission to automotive rear end. Minimum 2.50" x .083" or 2" x .120".

24.2 Quick-change rear ends allowed with steel tubes only.

24.3 Drive shaft tunnel must have minimum of 2 (3 recommended) 360-degree hoops between firewall and seat back. Must be fabricated of minimum .1875" X 1.5" steel bar. 1" X .095" steel tube highly recommended. If running a split drive shaft, a minimum of 3 (4 recommended) 360-degree hoops between firewall and seat back with 1 hoop being directly behind the centre bearing.

24.4 Rear must be locked or have a solid spool.

24.5 No slip spools allowed.

24.6 No titanium axles allowed

25 FRAME AND ROLL CAGE (Figure 14)

25.1 Main frame members will be minimum of 1" X 2" x .120" wall rectangular steel tubing

25.2 Main frame rail consist of the length between the front lower a-arm cross member and the rear main roll bar upright.

25.3 No aluminium allowed in cage or frame.

25.4 Main frame will have a minimum of 3 cross members between firewall and back of seat fabricated of same material as the frame rails.

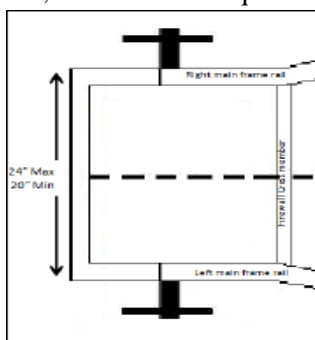
25.5 Alternate material may be steel angle, minimum .125" X 1.5" X 2" or 1 ½" DOM/CDS .095 martial non-seamed.

25.6 Cross-bracing in floor under driver strongly recommended. Rear frame rails at rear roll bar must be 34" max and 30" minimum outside dimension.

25.7 Rear roll bar must meet same dimension as the frame it is mounted to.

25.8 Frame may taper forward of driver seat area.

25.9 Front engine area frame width must be 24" max and 20" min outside dimension. (Cars manufactured prior to the 2018 production year with the narrow nose will be grandfathered). Cars manufactured after September 30, 2018 will be required to meet the 25.9 rule.



- 25.10** Left and right frame rails must be the same + or - 1/2"
- 25.11** 1" Right side raise rail permitted.
- 25.12** Roll cage must be DOM/CDS steel minimum 1.25" x .083" (.095 recommended).
- 25.13** Cross-brace or diagonal strongly recommended in hoop over driver's head.
- 25.14** All cars must have cage bars installed across door openings on both sides.
- 25.15** Bars should be welded in place as part of roll cage.
- 25.16** Minimum size 1" x .065". (1.25" x .095" recommended) cars with bars mounted on doors should have full doorframe and minimum of 2 horizontal cross bars. Minimum 1" x .083".
- 25.17** Cars with bars mounted on doors must have 1/4" x 2" steel stops welded in on each side of opening, totalling a minimum of 4" on each end of door.
- 25.18** It is required to have a steel bar with a minimum size of 1" X .065" from doorpost to doorpost, under the dash. Steel plate on driver's door bars recommended.
- 25.19** It is recommended that, if using rivets, body panels attach to tabs welded onto cage instead of drilling multiple holes into cage bars.
- 25.20** No down tube frame cars allowed. Any bars going from cowl area to snout must remain below the upper line of hood.
- 25.21** Classic pickups may have two (2) bars going from upper rear roll cage down to rear clip.

26 CHASSIS DIAGRAM (Figure 14)

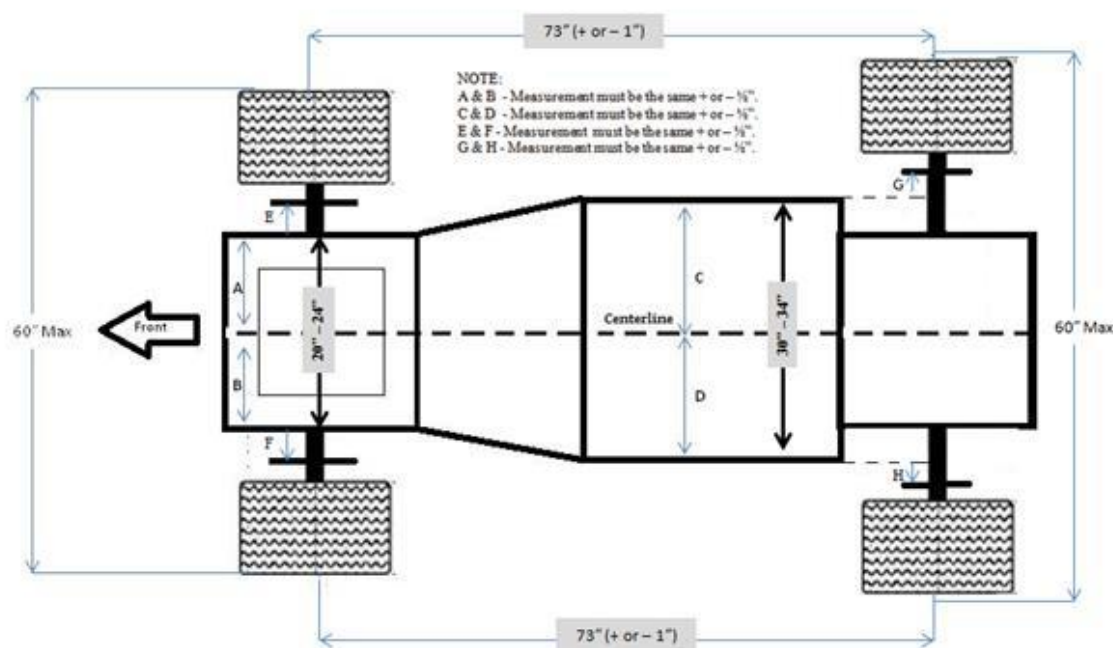


Figure 14

SUSPENSION & SHOCKS

27.1 Front suspension must not extend rearward past the most forward panel of the firewall.

27.2 Rear trailing arm/radius rod specifications:

27.2.1 Arms may have a maximum length of 30".

27.2.2 Torque-absorbing devices are permitted.

27.2.3 Maximum 3 radius rods (trailing arms) or 3 link suspension.

27.3 Wheel offsets are allowed.

27.4 Hub offsets are NOT allowed.

27.4.1 Hubs front and rear must be within 1/2" distance of each other from mainframe rail on either side of car, measured from centre of rotor to vertical plane of frame rail.

NOTE:

- Camber considerations must be taken.
- Make sure the rotor is completely parallel with the frame rail prior to taking the measurement.
- Measurement is made from the centre of the rotor close to the hub and centre of the rotor where it mounts to the hub, not at the outer or bottom edge of the rotor due to camber considerations.

27.5 Shocks must be of all-steel construction, non-adjustable type. (May not be of a type that allows disassembly).

NOTE:

Although not recommended we understand that our drivers have their shocks re-valve which requires special tooling and takes place off site from the racetrack. The intent of this rule is to reduce cost. The QA1 Twin Tube Shock or shock similar will not be allowed in any application under MODLITES AUSTRALIA rules

27.6 No driver shall have the ability to adjust suspension from inside car.

27.7 Straight front axles are prohibited.

27.8 No leaf springs.

27.9 No birdcage type rear linkage.

27.10 Anti-roll (sway) bars permitted front or rear. Must be connected without adjustments to arm location.

27.11 No torsion bar suspension system of any type allowed. No lift bars.

27.12 No cant-a-lever or cant-a-lever type device will be permitted in or on any area of the race car.

27.13 Coil-over shocks are the only allowable means of supporting the chassis. One shock and one spring per wheel only. Shock and spring must remain as one unit.

27.14 Schrader Valves or any custom modified valving that is similar will not be allowed in any application within the rules of MODLITES AUSTRALIA

28 Not suitable for Australian Standards

29 STEERING

29.1 All steering components must have safety fasteners such as cotter pins or self-locking nuts.

29.2 Manufactured quick-release steering wheel hubs mandatory.

29.3 Rack and pinion steering is mandatory.

30 BRAKES

30.1 All four wheels must have working callipers and rotors.

30.2 Rotors and callipers:

30.2.1 Steel rotors mandatory.

30.2.2 Steel OEM callipers or aluminium callipers.

30.2.3 Cutting and scalloping of rotors will be allowed. No surface cracks of any type will be allowed on the face of the rotors.

30.2.4 Drilling of rotors is permitted.

30.2.5 Adjustable brake bias and wheel shut offs allowed.

30.2.6 Competition-type master cylinders allowed.

31 SAFETY EQUIPMENT

31.1 Fire bottle system HIGHLY recommended must be fastened down with metal spring bands and not plastic. SFI 17.1

31.2 Fire resistant driving suit required. double layer SFI 3.2a5

31.2.1 Fireproof underwear required SFI 3.2

31.3 Fireproof gloves required. SFI 3.3/1 or 3.3/5.

31.4 Fireproof shoes required. SFI 3.3

31.5 Full-face helmet required. As per Speedway Australian rule no#**16.6**

31.6 Head and neck restraint systems mandatory SFI 3.38.1 Head and Neck Restraint Systems shall be inspected for recertification every five years after the date of original certification.

31.7 Driving racing harness & restraints required. SFI 16.1 as per Speedway Australia rule no# **14.1**

31.8 Seat must be high-backed aluminium, racing type. left and right head supports mandatory
Full Containment Seat recommended. SFI 39.2

31.9 Arm restraints SFI 3.3 mandatory:

31.9.1 Window nets SFI 27.1 recommended:

31.10 All competitors must have a fire extinguisher in their pit bays (*as per Speedway Australia rule #appendix F xix*)

32 WHEELS AND TIRES

32.1 Steel wheels only, minimum wall thickness .090", 13" only, Wheel width max. 8".

32.2 The official tire for Modlite Australia sanctioned events is the Hoosier 205/60-13, 215/60-13 235/60/13 soft and medium tire, other tires to be considered by events.

32.3 Hoosier Dirt Bozz Style tread pattern "V", Hoosier Racing Tires 40-durometer minimum before the race will remain in effect



33 1000cc MOTOR PROGRAM

33.1 GENERAL MOTOR SPECIFICATIONS

33.1.1 The intent of the stock engine is to use engines directly from a factory supplied street legal motorcycle, without any modifications whatsoever. If there is something that has not been addressed in the above specification, do not assume modifications can be made. You must request clarification from Modlites Australia with any item that has not been directly addressed.

33.1.2 Four cycle, four cylinders maximum only. Must have working transmission, clutch, and starter with all parts in place. Complete Clutch assembly must remain in engine. External reverse boxes allowed. No air controlled shifting devices, and no variable timing intake cam.

33.1.3 Engines must be front mounted in engine compartment.

33.1.4 Engine setback:

33.1.4.1 The "square of the engine" (rear fins of the jugs) may not extend rearward more than 17" from the centre of front spindle.

33.1.4.2 No part of the engine or transmission may extend rearward more than 21 inches from centre of front spindle.

33.2 ALLOWABLE 1000cc ENGINES MODEL

GSXR 1000 (2016 or older)

Honda CBR 1000 RR (2018 or older)

Yamaha YZFR1 (2019 or older)

Kawasaki ZX 10 (2016 or older)

33.3 ENGINE

33.3.1 Engine parts from different year engines can be swapped between engines providing they are from the same manufacture and same motor series.

33.3.2 No modifications to the bottom of the engine will be allowed, all bottom end components must remain in stock configuration.

33.3.3 Charging system optional.

33.3.4 Battery powered ignition system only.

33.3.5 Alternator cover allowed.

33.3.6 Factory balancer and/or balancer shaft maybe removed.

33.3.7 No auxiliary starters.

33.3.8 Engine must be cooled by original intent. May use extra fan or oil cooler.

33.3.9 Engine exhaust must include muffler, exit rearward and face downward on a 45° angle.
All muffler devices must include baffles. MUFFLER MUST MEET LOCAL TRACK REGULATIONS, (IF MANDATORY)

33.3.10 All head components must be OEM stock. After market cylinder heads and valves will not be permitted.

33.3.11 No porting, or grinding, polishing, or changing stock configuration of intake or exhaust ports allowed.

33.3.12 Engine must remain in its stock factory manufactured condition.

33.3.13 Bore and stroke must remain unaltered from a factory manufactured specification.

33.3.14 No increasing or decreasing of motor stroke in any way.

33.3.15 No engine can be modified to be less than original stock OEM configuration specifications.

33.3.16 Freshening of stock engine must only involve the replacement of any worn or faulty parts, with factory manufactured parts intended for use on that particular engine.

33.3.17 No altering of pistons, rods, head components. Crank, rods, and pistons must remain OEM.

33.3.18 Header must attach directly to head in stock exhaust port location.

33.4 CAMS

33.4.1 Cams must be as per original equipment manufacturer (OEM) and meet OEM specifications.

33.4.2 Optional adjustable cam sprockets permitted

33.4.3 Cam shafts must remain stock lift and duration and be in their original placement. (i.e. intake on intake side, and exhaust on exhaust side).

33.4.4 Kawasaki Racing Exhaust-Camshaft part number 49118-0729, Kawasaki Racing Intake-Camshaft part number 49118-0728, Kawasaki Cam Sprocket part number 120460034 are not permitted.

33.5 OIL SYSTEM

33.5.1 Oiling system may not include a dry sump system.

33.5.2 One-piece billet or cut down oil pans may be used.

33.5.3 High volume oil pump gear permitted.

33.5.4 The oil pan may be magnetic steel or aluminium.

33.6 FUEL SYSTEM

33.6.1 Engines must remain with the fuel injector or carburettor that was supplied with the engine from the factory. i.e. No interchanging a carburettor on an engine that came with a fuel injector.

33.6.2 No aftermarket fuel injection and turbo chargers.

33.6.3 Carburettor/fuel injection boot must fasten directly to head in the stock intake port location.

33.6.4 After market velocity stacks permitted

33.6.5 Factory supplied air boxes and or aftermarket air filters maybe used with OEM velocity stacks. After market air boxes, such as K & N or Kinser permitted

33.6.6 No ram air induction.

33.6.7 Fuel injection casting part lines must be visible upon inspection.

33.6.8 Intake port area may be altered to match intake boot. Alterations not to exceed 3/8" into intake port.

33.6.9 Only one fuel monitoring device can be used, but may not include any additional boxes, modules, or devices to alter motor function.

33.7 IGNITION SYSTEM

33.7.1 OEM stock ignition system only (Flashing of ECU/ECM acceptable). Power Commanders up to 5 USB and or TFI may be used. Oxygen sensors or components added will not be permitted. Components added to allow traction control will not be allowed. Bazzaz fuel performance devices will not be allowed.

33.7.2 Alternator recommended but not mandatory.

33.7.3 Wiring harnesses must be left exposed for easy inspection.

33.8 FUEL

33.8.1 Fuel: Gasoline/Petrol Pump Fuel. Premium Unleaded Petrol (PULP) 98 to be used
No Racing fuel, Ethanol, etc. or additives permitted.

33.8.2 No nitro or nitrous oxide.

33.9 OTHER

33.9.1 Engine must be in production for a designated time period to determine its competitive relationship with our current motor program.

33.9.2 Any major changes to the existing engine by the manufacturer must be reviewed by the MODLITES AUSTRALIA Officials before approval.

33.9.3 All electronic and/or computerized wheel spin and/or ignition retardation and/or acceleration limiting and/or traction control devices of any type will not be permitted.

33.9.4 At NO time will there be any type of ping control devices, dial a chip controls, timing controls, or any modifications to the ignition control boxes, distributors, or any other part of the Ignition System. This includes any add on component or components inside or outside the cockpit of any competitor's racecar.

33.9.5 There shall be NO driver-controlled wheel spin, timing, or fuel delivery control devices in the cockpit area of any racecar.

34 ENGINE TECH PROCEDURES

34.1 Carburetors or Fuel Injection and all 4 spark plugs must be removed before checking compression.

34.2 After ten (10) compression revolutions of cranking:

34.3 Cylinder cranking compression- minimum 100 p.s.i.

34.4 Maximum 220 p.s.i. per cylinder average of 4 cylinders, no variance

34.5 Compression to be taken within 15-45 minutes following feature event

34.6 Measured with a Snap-On Motorcycle compression gauge, part number EEPV303A: with adapters EEPV304A, EEPV306A or MT26J200, EEPV305A or with Snap-On Motorcycle Compression Gauge Set EEPV503: with adapters EEPV5G10L, EEPV5G12L and EEPV5G14L.

35 ENGINE COMPLIANCE CHECKS

35.1 The top 5 placegetters in National and State title's engines will be sealed and the top 3 engines will be stripped and measured for compliance by the states authorised inspection agent at the discretion of Modlites Australia Officials.

35.2 Any other engine inspections will be at the discretion of State and National clubs when deemed necessary or as per General Rules **no# 21 PROTESTS**.

35.3 Cost of engine compliance checks will be by the State requesting the compliance check.

35.4 Any engine found to be out of compliance will be deemed illegal and comply to General Rule **no#10.1.1**

35.5 If a car owned wishes to have their engine checked and sealed prior to the season by the states authorised agent (*at their expense*) that engine will not be required to be re-checked at any title or meeting.

35.5.1 There must be 2 seals on these engines 1 as per rule **no# 13.1** and 1 between two cam cover bolts.

35.5.2 If at any time the seals are broken and not re-sealed by the authorised agent that engine will then become subjected to rule no# **35.1 & 35.2**

35.6 MA has the right to check any engine at its discretion.

36 WEIGHT

36.1 All cars following “1000cc Motor Program” must weigh a minimum of 1325 lbs, with driver after race.

36.2 No weight to be attached outside of car body, i.e. on nerf bars.

36.3 This weight program will be monitored for parity and competitiveness and may be changed at the discretion of MODLITES AUSTRALIA

36.4 Attached weight must be securely bolted to frame with 2 (1/2") or larger bolts with a minimum 30mm washer up to a maximum of 11.5kg per single weight. Weight must be painted white with car number displayed. Penalty for losing attached weight on the racetrack is disqualification from the event.

NOTES

Weight allocations for all engine programs can be reviewed by the MA, based on track performance, and may be altered to suit the competitive nature of Modlite racing within Australia.



2020/21 **MODLITES AUSTRALIA INC.**

GENERAL RULES

July 1, 2020

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1 PREFACE

The rules and/or regulations set forth herein are designed to provide for the orderly conduct of racing events and to establish minimum acceptable requirements for such events. These rules shall govern the condition of all events, and by participating in these events, all participants are deemed to have complied with these rules. No expressed or implied warranty of safety shall result from publications of or compliance with these rules and/or regulations. They are intended as a guide for the conduct of the sport and are in no way a guarantee against injury or death to a participant, spectator or official. The steward shall be empowered to permit reasonable and appropriate deviation from any of the specifications herein or impose any further restrictions that in his opinion do not alter the minimum acceptable requirements. No expressed or implied warranty of safety shall result from such alteration of specifications. Any interpretation or deviation of these rules is left to the discretion of the officials and their decisions are final. These rules pertain to all Modlite Australia and State associated clubs. These rules are to be read in conjunction with Speedway Australia rules and regulations.

2 RULES REVIEW PERIOD

- 2.1** Clubs and car owners are encouraged to participate during the rules review period of 1st of November to 31st of December.
- 2.2** All rule change submissions are to be forwarded on the correct rule change submission forms only
- 2.3** Technical committee to check feasibility of all rule change submissions and submit recommendations to MA
- 2.4** Send request and recommendations to modlitesaustralia@outlook.com

3 GENERAL REGULATIONS

3.1 The rules and/or regulations set forth herein are designed to provide for the orderly conduct of racing events and to establish the minimum acceptable requirements for such events. These rules shall govern the condition of all events, and, by participating in these events, all participants are deemed to have accepted and complied with these rules. **NO EXPRESSED OR IMPLIED WARRANTY OF SAFETY SHALL RESULT FROM PUBLICATION OF OR COMPLIANCE WITH THESE RULES AND REGULATIONS.** They are intended solely as a guide for the conduct of the sport and are in no way a guarantee against injury or death to a participant, spectator, or official. Any interpretation or deviation of these rules is left to the discretion of Modlite Australia Officials and the decision is final. **NO EXPRESSED OR IMPLIED WARRANTY OF SAFETY SHALL RESULT FROM SUCH DEVIATION OF SPECIFICATIONS.**

3.2 No intoxicating spirits or controlled substances are to be consumed before or during race events by anyone entering the pit area of tracks.

3.3 Abuse or confrontation of any official and/or use of abusive language is strictly prohibited. **FAILURE TO ABIDE BY THIS RULE MAY RESULT IN SUSPENSION, LOSS OF POINTS, WINNINGS, RECORDS, AND/OR CASH FINE.**

3.4 Any disputes involving qualifying times, lap scoring, finishing position is under the jurisdiction of the hosting track officials. Modlite Australia officials will address winnings, points, or rules interpretations. Any disputes or discussions of this nature are to be held with Modlite Australia officials only.

3.5 Drivers, owners, and/or crew shall have no claims or actions of any kind against or cause of action for damages of any kind, expenses, or otherwise against Modlite Australia, any track owner, promoter, or official by reason of disqualification or damage to either car, driver, or both. This regulation shall be deemed a covenant not to sue made by each driver, owner, and/or crew member to Modlite Australia, any track owner, promoter, or official with respect to any alleged act or omission or agreement by any of him or her.

3.6 Modlites Australia or track officials may inspect any car at any time for safety, mechanical, or rules compliance.

3.7 ANY RULES OR REGULATIONS NOT COVERED IN THIS BOOKLET WILL BE DETERMINED ON A TIMELY BASIS BY MODLITES AUSTRALIA AND THE DECISION IS FINAL.

3.8 MOTOR RACING IS A DANGEROUS SPORT AND MAY RESULT IN INJURY, BODILY HARM, OR DEATH.

3.9 The steward shall be empowered to permit reasonable and appropriate deviation from any of the specifications herein or impose any further restrictions that in his opinion do not alter the minimum acceptable requirements. No expressed or implied warranty of safety shall result from such alteration of specifications. Any interpretation or deviation of these rules is left to the discretion of the Modlites Australia officials and their decisions are final.

3.10 Points go to the driver only. No exceptions.

3.11 Feature events will be 15, 20, or 25 lap events. At all regular Modlites Australia events. Heat races will be 8 laps in length with B-Mains 10-12 laps. Due to weather, time restraints or car numbers laps maybe altered.

3.12 All restarts will be as per Speedway Australia rule No# 4.13.

3.13 In cases where the finish of the race is too close to call (in real time), the track officials may use photo(s), video or electronic scoring, if available, to aid in determining the winner.

3.14 Modlites Australia officials and the Speedway Australia appointed Steward of the meeting have final say on all official calls and rules.

4 LINE-UP PROCEDURES

4.1 Draw

The draw for positions will be done either at the track directly after the drivers briefing or at the discretion of Modlite Australia or State Associations.

4.2 Heats

Heats will be lined-up as per the draw. Cars must start in their scheduled heat.

4.3 Features:

The field will be filled in a straight-up manner by the drivers heat race finishing positions as deemed by the point scores. (clubs may change formats to best suit local club run events)

5 ORDER OF EVENTS

5.1 The order of events and line ups at each individual track will be posted on a white board which will be located in the pit area at all tracks. It is the responsibility of each individual race team to stay abreast of changes to those line-ups.

6 WHITE FLAG RULE

6.1 All cars will follow the White Flag rule when lining up for heats, B-Mains, and features. The White Flag rule means that all cars will be in the designated line-up area before the white flag is waved in the previous heat race, B-Main or Feature event, or said cars will start at the rear of the field unless due to mechanical reasons the 2-minute rule is requested.

7 DRIVER'S MEETING

7.1 All drivers must attend Modlites Australia Driver's Briefing. If a driver does not attend the driver's briefing, they will automatically start from the rear of all their heats for that meeting.

8 BLUE RIBBON EVENTS

8.1 To be eligible to compete in any blue-ribbon Modlites Australia sanctioned event (*State or National*) the driver must have competed in at least 3 Modlites Australia sanctioned competition events of that current season.

8.2 Exemptions can be requested in writing to the Modlites Australia Secretary and the decision to allow, or not, to enter the event will be at the discretion of the Modlites Australia committee.

9 AUSTRALIAN TITLE FORMAT

9.1 A 4-lap time trial will be conducted for heat starting positions **only** points do not count, **or** at the discretion of Modlites Australia officials and the track promotor a seeded draw to be done prior to the meeting.

9.2 Draw for group starting position will done prior to the meeting.

9.3 Heats will be run over 8-10 laps. The final will be run over 25 laps.

9.4 The meeting will consist of a minimum 4 heats per competitor, a top 8 shootout **or**, "B" main, and an "A" main.

9.5 The shootout will consist of the highest 8 qualifiers in a 4 lap race.

9.6 The "A" main will consist of the top 16 - 20 qualifiers plus the top 4 from the "B" Main If any of the qualifiers are unable to take their place in the final or, due to a stoppage within the first 2 laps a qualifier is unable to continue, then the reserves (5th or 6th from "B"main) can fill the last few grid positions.

9.7 Prize money for the Australian Modlite Title can be no less than the previous year and can only be raised with approval from Modlites Australia Committee.

9.8 Any variation of the format will be at the discretion of the Modlites Australia Committee.

10 GENERAL GUIDELINES FOR CONDUCT PERTAINING TO RULES INFRACTIONS

10.1 MOTOR INFRACTION

10.1.1 Anyone found to be out of compliance with any of the current motor rules (bore, stroke, illegal head modifications, etc. as per current 2020/21 MODLITES AUSTRALIA rule No# 33.3) will be assessed the following fines and penalties:

Competitor (driver) will be suspended for the remainder of the season, MODLITES AUSTRALIA membership will be forfeited, and all points for the season will be expunged from the records. In the event that a motor is found to be out of compliance, the motor numbers will be noted, and that motor will be henceforth no longer deemed legal to run at any MODLITES AUSTRALIA race regardless of whether it has been reconfigured to be legal or not.

10.2 OTHER INFRACTIONS (Weight, Wheelbase, Parts, etc.)

10.2.1 Anyone found to be out of compliance with the rules of driver and car overall weight requirements, wheelbase, illegal suspension, weight, safety, etc. (as per current rule book for the MODLITES AUSTRALIA 2020/21 racing season) will be given the following penalties:

10.2.1.1 FIRST OFFENSE: Forfeit of points and monies for the race program of record and warning to have the violation corrected before being allowed to compete at the next event.

10.2.1.2 SECOND OFFENSE: Driver will incur the same fines and penalties as first offense but will subsequently earn an additional race suspension.

10.2.1.3 THIRD and SUBSEQUENT OFFENSES: Driver will incur same fines and earn an additional 2 race suspension and forfeit all points earned to date.

11 REFUSAL TO SUBMIT TO TECH PROCEDURES

11.1 Any driver or car owner that refuses to have their car tech'd by Modlites Australia will result in the car and driver being suspended from competition until a meeting has been convened to discuss the matter. All decisions are binding and final, no appeals or changes may be made after the issue is settled. Penalties can be given to drivers and owners.

11.2 Race cancellations will not be counted towards the suspension period earned by the offending driver.

12 RACERS' CODE OF CONDUCT

12.1 DISCIPLINE

All drivers entering a Modlites Australia event shall be subject to discipline for any violation of the rules and regulations; violation of the Racer's Code of Conduct; or the failure to obey the directives and decisions of the Modlites Australia Officials, Promoter and track officials.

12.2 DRIVERS RESPONSIBLE

The drivers shall be responsible, at all times, for the behaviour of the owner of the car and all pit crew and discipline may be imposed upon the driver for any car owner's or pit crew member's violation of the rules and regulations; violation of the Racer's Code of Conduct; or the failure to

obey the directives and decisions of the Modlites Australia Officials. However, Modlites Australia may also impose discipline upon the offending car owner or pit crew member as Modlites Australia Officials deem appropriate in their sole discretion.

12.3 GOOD CONDUCT REQUIRED

All drivers, car owners and pit crew members shall at all times conduct themselves in a manner that reflects favourably upon the sport and Modlites Australia. Any failure to adhere to this rule may result in the imposition of discipline, in the sole discretion of the Modlites Australia.

12.4 NO DRUGS

As per Speedway Australia rule No# 6.1, 6.2, 6.3

12.5 NO ALCOHOL

As per Speedway Australia rule No# 6.1, 6.5

12.6 PERSONS UNDER THE INFLUENCE

As per Speedway Australia rule No# 6.1, 6.4

12.7 SOCIAL MEDIA

As per Speedway Australia Social Media Policy

12.8 FIGHTING OR OTHER MISCONDUCT

Any person taking part in discourteous conduct or causing a disturbance is subject to discipline. Persons leaving their pit area to go to another pit area or to any other location to cause a disturbance are subject to discipline. If any physical contact occurs, including but not limited to fighting, pushing, or shoving, the offender is subject to discipline which shall include the loss of accumulated points and a fine and suspension, the amount and duration of which shall be determined by Modlites Australia Officials in their sole discretion. Repeat offenders shall be subject to progressive discipline (which may include a lifetime ban). Modlites Australia disapproves of this sort of behaviour because it is detrimental to the sport and the presentation of a professionally staged entertainment program suitable for the entire family.

12.9 ROUGH DRIVING

It is the policy of Modlites Australia to provide good, clean, competitive racing. Any driver, in the opinion of the Modlites Australia Officials, charged with rough driving (overly aggressive driving) will be subject to discipline, which shall include a fine, forfeiture of points, and suspension. Repeat offenders shall be subject to progressive discipline (which may include a lifetime ban). This rule against rough driving applies to both on-track incidents and those which occur in the pits. Modlites Australia strongly disapproves of rough driving because it presents a safety hazard for drivers, spectators, and others; because it is detrimental to the sport; and because it interferes with the presentation of a professionally staged entertainment program suitable for the entire family.

12.10 NO ABUSIVE LANGUAGE

No driver or other person affiliated with the car (i.e., owner, pit crew member) shall subject the Modlites Australia Officials, scorers, or speedway officials to verbal (improper/obscene/profane language) or physical abuse at any time. Offenders shall be subject to discipline, which shall include a fine, forfeiture of points, and suspension, and repeat offenders shall be subject to progressive discipline. Modlites Australia disapproves of this sort of behaviour because it is detrimental to the sport and the presentation of a professionally staged entertainment program for the entire family.

12.11 NO UNSPORTSMANLIKE CONDUCT

No driver shall engage in any unsportsmanlike conduct. Modlites Australia Officials have the sole discretion to determine what constitutes unsportsmanlike conduct.

13 TECHNICAL INSPECTIONS

13.1 ENGINE COMPLIANCE INSPECTION REQUIREMENTS

13.1.1 Each State will select two (2) qualified engine builders to do engine compliance inspections.

13.1.2 A register of selected engine builders will be held by the National Secretary.

13.1.3 Engine inspections will be at the discretion of State and National clubs when deemed necessary or as per rule **no# 21 PROTESTS**.

13.1.4 Cost of engine compliance checks will be by the State requesting the compliance check.

13.1.5 Any engine found to be out of compliance will be deemed illegal and comply to rule **10.1.1**.

13.2 RIGHT TO INSPECT

Modlites Australia Officials may inspect any car at any time before, during or after the race.

13.3 PRIOR INSPECTIONS

The fact that a car was inspected and determined to be legal or safe on any prior occasion shall not govern. Cars must be legal and safe at all times. Cars deemed to be illegal or unsafe shall not be allowed to compete in the event and the driver may be subject to discipline. Modlites Australia Officials make no warranty or representation of the legality or safety of cars allowed to compete in the event.

13.4 POST-RACE INSPECTIONS

If any illegality is detected in any post-race inspection, the car will be disqualified, relegated to last place in the official order of finish for the race in which the disqualification occurred and the driver may be subject to discipline, which may include but is not limited to forfeiture of purse and/or points.

14 WEIGHING OF CARS

14.1 SCALES

All drivers must cross the scales if requested over the one-way radios. Any car that goes to its' pit area before going to the scales will be presumed to be too light and will be disqualified from that race without right of protest or appeal.

14.2 WEIGH-IN PROCEDURE

The car must approach the scales at a safe speed, ease onto the scales, come to a complete stop, and let the engine idle. If the car is not positioned properly, it shall ease off the scales and repeat the procedure. The driver shall refrain from excessive motion while the car is on the scales and shall not rock or bounce the car. The driver shall not rev the motor. When instructed, the car shall ease off the scales. Failure to comply with any portion of this procedure shall be deemed to be unsportsmanlike conduct and the driver may be subject to discipline which may include disqualification without right of protest or appeal.

14.3 CAR DETERMINED LIGHT

If a car is determined to be too light after the race is over the car shall be reweighed. The Modlites Australia Officials, in their sole discretion, shall determine the best course of action for reweighing. Locations of the scales, proximity of the line of cars, are but two factors in that determination. The Modlites Australia Officials may simply back the car back up onto the scales and reweigh. The car shall repeat the weigh-in procedure. If the car is determined to be too light again, it will be disqualified from that race and relegated to last place in the official order of finish, all without right of protest or appeal. If the car makes the minimum weight the second time it is weighed, it will be deemed to be legal. No car will be weighed more than two (2) times. If the driver goes to his pit area or onto the track, it will be presumed that the car is too light, and it will be disqualified from that race and relegated to last place in the official order of finish, all without right of protest or appeal, and the driver may be subject to discipline.

14.4 DRIVER'S RESPONSIBILITY

It is the driver's responsibility to be sure that the car meets the minimum weight at all times. The driver may request that the car be weighed at any time before or during the program, so long as the request does not interfere with the orderly presentation of the program.

14.5 EARLIER WEIGHT CHECKS

Cars may be weighed several times over the course of the racing program. Just because the car made minimum weight earlier in the program will not be controlling any other time the car is weighed.

14.6 NO TOLERANCE

Modlites Australia does not allow for any tolerances for fuel burn-off or any other reason.

14.7 SCALES ARE OFFICIAL

All scales used by Modlites Australia are considered to be the official scales for that racing event only. The scales might not be calibrated or certified to be considered official.

15 ALL DECISIONS FINAL

15.1 The decisions of the Modlites Australia Officials concerning compliance with the weight requirements and procedures shall be final and shall not be subject to protest or litigation.

16 DISCIPLINE

16.1 RIGHT TO DISCIPLINE

Modlites Australia Officials shall have the sole right to determine whether a driver or other person shall be subject to discipline.

16.2 WHETHER TO IMPOSE DISCIPLINE

Whether a person is subject to discipline shall be a matter entrusted to the discretion of the Modlites Australia Officials.

16.3 AMOUNT OF DISCIPLINE Unless rules of Modlites Australia establish a specific penalty for any transgression, the nature and amount of the discipline imposed shall be within the sole discretion of the Modlites Australia Officials.

16.4 FORMS OF DISCIPLINE

Modlites Australia Officials may impose any of the following forms of discipline, singly or cumulatively:

16.4.1 Disqualification from the event.

16.4.2 Forfeiture of points for the event or the season to date.

16.4.3 Forfeiture of purse money or contingency money.

16.4.4 Forfeiture of point money, if any.

16.4.5 Imposition of a fine.

16.4.6 Suspension from one or more future events.

16.4.7 Lifetime ban from all Modlites Australia sanctioned events.

16.5 PROGRESSIVE DISCIPLINE

Modlites Australia shall keep a record of all discipline imposed. Persons subject to progressive discipline shall suffer a greater penalty for the second and each subsequent offense. For example, if a driver is fined \$100 for the first offense, the second offense may result in a fine of \$200 and the third offense may result in a fine of \$300. Progressive discipline; however, shall not carry over to the following season.

17 FINES

17.1 All fines imposed by Modlites Australia before the pay-off is made shall be withheld from the purse or contingency monies the driver is eligible to receive for the event. If the purse or contingency monies due the driver are not sufficient to satisfy the fine, the fine will carry forward to subsequent Modlites Australia events. If the fine is imposed after the pay-off is completed for that event, it will carry forward to subsequent Modlites Australia s events. A driver must pay all outstanding fines to be allowed to enter any subsequent Modlites Australia events. If an owner/driver does not pay a fine imposed, the owner/driver's car cannot compete in a subsequent Modlites Australia event with another driver until the fine is paid. A car owner must pay all fines before the car may be driven in any subsequent Modlites Australia event. Any fines remaining unpaid at the end of the season shall be withheld from the driver's point money, if any is due the owner/driver. If the point money is insufficient to pay the outstanding fine or if the driver is not eligible for any point money, the fine shall carry over from season to season until it is fully paid.

18 SUSPENSIONS

18.1 All suspensions must be fully served. Cancelled events do not count against the suspension. For example, if a driver is suspended for two (2) events and the next event is cancelled due to inclement weather, the suspension continues in effect until fully served. Any suspension imposed shall carry over to the following season until fully served. For example, if a driver is given a three-race suspension but only two events remain on the schedule, the suspension will still be in effect at the first event of the following season.

19 FINALITIES OF DECISION

19.1 The decisions of the Modlites Australia Officials regarding discipline shall not be subject to protest or litigation.

20 NOT LAW ENFORCEMENT

20.1 While Modlites Australia has the right to impose discipline for any misconduct that is in violation of any law (such as assault, terroristic threats, public intoxication or illegal drug use), the Modlites Australia Officials are not responsible for law enforcement or investigation. Rather, investigation and prosecution shall be the responsibility of law enforcement officials and/or speedway security. Modlites Australia Officials shall cooperate whenever practicable but are not bound by any decision of law enforcement and/or security to undertake or to decline an investigation or prosecution.

21 PROTESTS

21.1 SPOKESMAN

The driver or car owner shall be the sole spokesman for the team.

21.2 VISIBLE PROTESTS

Protests may be made concerning visible violations of rules or specifications. A visible protest situation is any time a mechanical device is not needed by a Modlites Australia Official to determine the car's compliance with the rule.

21.3 TECHNICAL PROTESTS

21.3.1. PROTEST FEES:

- a) All fees must be paid in cash.
- b) Fuel Injection inspection \$50.00. No MA fees.
- c) \$500.00 Motor tear down top half of engine including head, cam, and valve. MA retains \$250.00.
- d) \$1,250.00 complete motor tear down top and bottom. MA retains \$500.00.

21.3.2 TECHNICAL PROTESTS STEPS

21.3.2.1. A technical protest situation is any protest which requires any tool or mechanical device (including scales) for Modlites Australia Officials to determine a car's rule compliance.

21.3.2.2. Technical protests must be filed with the Modlites Australia Official and must be accompanied by the protest fee, paid in cash prior to the inspection. Failure to pay the protest fee, in cash, shall be deemed a waiver of the right to protest and shall preclude any subsequent appeal or litigation.

21.3.2.3. For the purpose of a subsequent appeal, all measurements by Modlites Australia Officials are presumed to be correct, absent a showing by the driver of a mistake or prejudice.

21.3.2.4. If the protested car is found to be illegal, it shall be the burden of the driver to establish legality in any subsequent appeal or litigation.

21.3.2.5. If the protested car is found to be illegal, the protest fee is refunded to the protester, less an administrative fee, which shall be retained by the Modlites Australia and used for the cost of the tear down. If the car is found to be legal, the fee is retained by Modlites Australia, with the difference going to the protested car. Cars that are found to be illegal will forfeit all points earned for the race in questions and the driver will also forfeit any earnings due for that race. Modlites Australia will use the administrative fee portion to offset the labour cost of the tear down.

21.3.2.6. Modlites Australia shall not be responsible for any damage caused by the inspection.

21.4 RACE RESULTS PROTESTS

Any affected driver may protest race results within 10 minutes after the race. A recheck of scoring will then be made, if necessary, and, thereafter, the results will be considered final.

21.5 HOLDING THE PURSE

In the event of a protest, the speedway may, with the advice and consent of Modlites Australia, delay purse distribution until the matter is ultimately decided by the appeal board. The speedway can, at its discretion and with the advice and consent of Modlites Australia, distribute that portion of the purse to drivers whose finishing positions will not be affected by the protest or any subsequent appeal.

21.6 RIGHT TO REFUSE PROTEST

Modlites Australia reserves the right to refuse any protest if the protest is deemed unnecessary and/or is deemed to have been used for purposes of harassment.

21.7 RECORDS OF PROTEST

Modlites Australia will retain all written protests for seven (7) days after the event giving rise to the protest.

22 APPEALS

22.1 EXCLUSIVE REMEDY

MODLITES AUSTRALIA has established a grievance board of appeals to rule on appeals. The appeal process contained in these rules shall be binding on all parties and shall not be subject to judicial review or litigation. The appeal process is the exclusive means for any aggrieved party to seek review of any reviewable decision of a Modlites Australia Official.

22.2 SUBMITTING APPEAL

The appeal must be submitted, in writing, to the Modlites Australia Office or to a Modlites Australia Official, with a copy to the Secretary, Modlites Australia by email to modlitesaustralia@outlook.com, within 48 hours of the event or decision giving rise to the appeal. The appeal will be deemed submitted on the date of actual receipt by the Modlites Australia secretary; but if the appeal is submitted by mail, then the date of the postmark shall govern. The appeal must state the following: the decision to be reviewed; the date of the event or decision giving rise to the appeal; the name of the Modlites Australia Official, if any, involved in the matter; the substance of the decision to be reviewed; and specific grounds for which an appeal is sought. The failure to submit a timely, written and specific appeal shall be deemed to be a waiver of the right to appeal and shall not be subject to judicial review or litigation.

22.3 APPEAL BOARD

Members of the appeal board shall be determined as follows: Two (2) members of the board shall be selected by the Modlites Australia Committee; one (1) member of the board shall be selected by the driver lodging the appeal, with the following provision: the driver representative shall not have any interest whatsoever in the outcome of the appeal. The Chairman of the Appeal Board shall be an administrative member, but the chairman or the designee shall have no vote, except to break ties.

22.4 CONVENING THE BOARD

The appeal board will meet whenever and wherever necessary for the efficient and speedy disposition of its business. The appealing party will be given written notice of the date, time and place for the meeting. The meeting may take place in person or by telephone. A quorum of the board shall consist of any two voting members of the appeal board and the chairman or the chairman's designee.

22.5 DECISION

A majority of the members of the board voting must concur to modify any penalty or determine any appeal. A decision of the majority of the board shall be considered final and shall not be subject to any judicial review or litigation. If the board fails to agree on action to be taken on an appeal, the appeal shall be referred to the Modlites Australia president for decision and the president's decision shall be final and shall not be subject to judicial review or litigation.

22.6 PARTS

When an appeal involved the legality of any parts, the affected driver or car owner must surrender the part or parts in question. Modlites Australia will issue a receipt upon surrender of the parts, but the parts will not be returned until after the appeal hearing is concluded.

22.7 APPEAL PROCEDURE

22.7.1. The decision of the Modlites Australia Official or Officials shall be put into the record.

22.7.2. The contents of the written appeal shall be put into the record.

22.7.3. The aggrieved party will state their case and will call any witnesses or introduce any evidence to support that case. Hearsay evidence is non-admissible.

22.7.4. The Modlites Australia Official(s) will state the case in support of the decision from which an appeal is being made and may call any witnesses or introduce any evidence to support that case. Hearsay evidence is non-admissible.

22.7.5. The aggrieved driver will make any desired rebuttals, additions to the record or summations.

22.7.6. The Modlites Australia Official(s) will make any desired rebuttals, additions to the record or summations.

22.7.7. At any time during the hearing, members of the appellate board will be given the opportunity to ask any questions of any person present in person or by telephone at the time of the hearing.

22.7.8. In the interest of justice, the appeal board may continue any hearing to receive additional evidence.

22.7.9. After the record is closed, the members of the appeal board will deliberate in private and will make its determination, decision, or recommendation by majority vote.

22.7.10. All interested parties will be notified in writing of the appeal board's decision.

21.7.11. Modlites Australia shall retain copies of all decisions by the appeal board.

22.7.12. If the payment of the purse or any portion thereof was withheld due to the appeal, the promoter shall be notified, in writing, by Modlites Australia of the decision and its impact upon the payment of the purse and the promoter shall distribute all withheld sums forthwith.

23 MODLITES AUSTRALIA NATIONAL/STATE POINTS

23.1 Only Modlites Australia registered drivers can accumulate points for the 2020/21 season at any Modlites Australia sanctioned event.

23.2 Point structure

Position	Points	Position	Points	Position	Points	Position	Points	Position	Points
1 st	120	10 th	92	19 th	74	28 th	58	37 th	48
2 nd	114	11 th	90	20 th	72	29 th	57	38 th	47
3 rd	109	12 th	88	21 st	70	30 th	56	39 th	46
4 th	105	13 th	86	22 nd	68	31 st	55	40 th	45
5 th	102	14 th	84	23 rd	66	32 nd	54	41 st	44
6 th	100	15 th	82	24 th	64	33 rd	53	42 nd	43
7 th	98	16 th	80	25 th	62	34 th	52	43 rd	42
8 th	96	17 th	78	26 th	60	35 th	51	44 th	41
9 th	94	18 th	76	27 th	59	36 th	49	45 th	40

24 RULE CLARIFICATIONS

24.1 Any rules or regulations not covered in this document will be determined on a timely basis and voted on by Modlites Australia committee members and by Modlites Australia Officials and those decisions will be final.